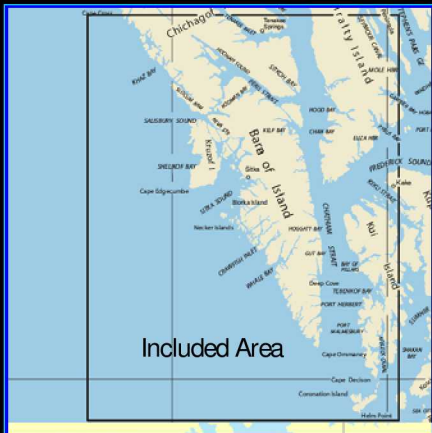


BookletChartTM

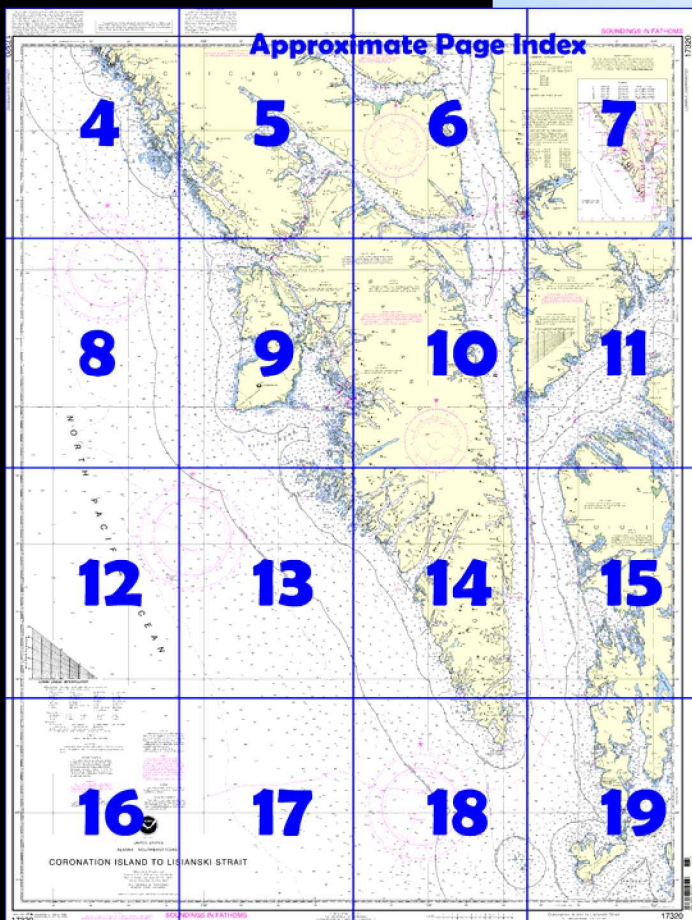
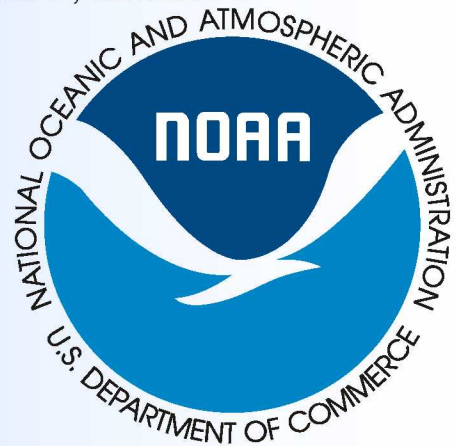
Coronation Island to Lisianski Strait

(NOAA Chart 17320)



A reduced scale NOAA nautical chart for small boaters. When possible, use the full size NOAA chart for navigation.

- ✓ Complete, reduced scale nautical chart
- ✓ Print at home for free
- ✓ Convenient size
- ✓ Up to date with all Notices to Mariners
- ✓ United States Coast Pilot excerpts
- ✓ Compiled by NOAA, the nation's chartmaker.



Home Edition (not for sale)

What are Nautical Charts?

Nautical charts are a fundamental tool of marine navigation. They show water depths, obstructions, buoys, other aids to navigation, and much more. The information is shown in a way that promotes safe and efficient navigation. Chart carriage is mandatory on the commercial ships that carry America's commerce. They are also used on every Navy and Coast Guard ship, fishing and passenger vessels, and are widely carried by recreational boaters.

What is a BookletChart™?

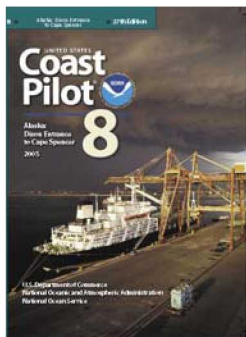
This BookletChart is made to help recreational boaters locate themselves on the water. It has been reduced in scale for convenience, but otherwise contains all the information of the full-scale nautical chart. The bar scales have also been reduced, and are accurate when used to measure distances in this BookletChart. See the Note at the bottom of page 5 for the reduction in scale applied to this chart.

Whenever possible, use the official, full scale NOAA nautical chart for navigation. Nautical chart sales agents are listed on the Internet at <http://www.NauticalCharts.NOAA.gov>.

This BookletChart does NOT fulfill chart carriage requirements for regulated commercial vessels under Titles 33 and 44 of the Code of Federal Regulations.

Notice to Mariners Correction Status

This BookletChart has been updated for chart corrections published in the U.S. Coast Guard Local Notice to Mariners, the National Geospatial Intelligence Agency Weekly Notice to Mariners, and, where applicable, the Canadian Coast Guard Notice to Mariners. Additional chart corrections have been made by NOAA in advance of their publication in a Notice to Mariners. The last Notices to Mariners applied to this chart are listed in the Note at the bottom of page 7. Coast Pilot excerpts are not being corrected.



[Coast Pilot 8, Chapter 8, 10 & 11 excerpts]

(2) **Frederick Sound** has its entrance from Chatham Strait between Kingsmill Point and Point Gardner and extends NE to The Brothers and Cape Fanshaw, at the entrance to Stephens Passage, and SE to Dry Strait, a high-water boat passage connecting it with the E end of Sumner Strait.

(2) **Chatham Strait** is the most extensive of the inland passages of southeastern Alaska. It is about 18 miles wide at its entrance between

Cape Ommaney and Coronation Island and about 13.5 miles between the cape and the W shore of Kuiu Island, with a length of 138 miles from Coronation Island N to Rocky Island. The main strait is clear, open, and deep throughout, but some of the bays and bights are foul. In the winter, ice forms in many of the bays and inlets, particularly those into which large freshwater streams empty and which have narrow entrances. The W

shore as far as Point Augusta is high, bluff, and rugged, and free from hidden dangers in the way of navigation from point to point, except in the vicinity of the E entrance to Peril Strait. The water is shoaler on the E side, and the reefs extend out farther, but in most cases they are in the bights and bays, and in no case do they extend beyond a line drawn 0.5 mile off from point to point, except a ledge about 1 mile offshore at Point Crowley.

(2) **Baranof Island**, about 90 miles long with a greatest width of about 22 miles, forms about one-third of the outer coastline of southeastern Alaska between Cape Muzon and Cape Spencer. The W coast from Cape Ommaney at Chatham Strait to Point Kakul at Peril Strait is about 80 miles. **Mt. Katlian**, 4,303 feet high, is in the N part of the island. The greater elevations are on the S part of the island.

(12) **Hazy Islands** form two distinctive groups about 8 miles W of Coronation Island and are separated by a channel 1.2 miles wide with depths of 25 to 40 fathoms. The NW group consists of three prominent islets; the largest is **Big Hazy Islet**, 258 feet high and conical.

(17) **Cape Ommaney**, the W point at the entrance to Chatham Strait, is a remarkable promontory terminating in Ommaney Peak, a bluff, rugged, rocky 2,005-foot-high mountain, detached from the higher land N by a low depression running through from Port Conclusion. **Wooden Island**, sparsely wooded, is close SE of Cape Ommaney.

(18) **Cape Ommaney Light** (56°09'37"N., 134°39'40"W.), 195 feet (59.4 m) above the water, is shown from a post on Wooden Island. This light, together with Helm Point Light, Point Crowley Light 2, and Cape Decision Light, marks the approach to Chatham Strait..

(45) **Port Malmesbury** is on the E side of Chatham Strait, 17 miles N of Cape Decision.

(58) **Tebenkof Bay** is on the E side of Chatham Strait N of Gedney Harbor. Its entrance is between Point Ellis on the N and **Swaine Point** on the S.

(73) **Point Ellis** (56°33.8'N., 134°19.2'W.), the S point of Bay of Pillars, is 16.5 miles N of Point Harris. A bare reef is 0.4 mile WNW of Point Ellis. The reef is marked by **Point Ellis Light** (56°34'00"N., 134°19'59"W.).

(91) **Kingsmill Point** is on the E side of Chatham Strait at its junction with Frederick Sound. It is marked by **Kingsmill Point Light** (56°50'36"N., 134°25'12"W.).

(93) **Port Alexander**, indenting the W shore of the strait about 5 miles N of Cape Ommaney, is a small-boat harbor with an entrance 150 yards wide.

(94) **Port Alexander Light** (56°14'23"N., 134°38'59"W.), 68 feet (20.7 m) above the water, is shown from a small house on a skeleton tower with a red and white diamond-shaped daymark on the high bare rocky point at the S side of the entrance.

(95) **Sitka Sound** has its entrance from the sea between Biorka Island and Cape Edgecumbe. It extends in a N direction about 16 miles, with a width E and W of 5 to 10 miles.

(96) **Cape Edgecumbe**, about 64 miles NW of Cape Ommaney, is the SW extremity of **Kruzof Island**.

(111) **Port Lucy** has its entrance on the W shore about 10.5 miles N of Cape Ommaney.

(112) **Toledo Harbor** is a small, horseshoe-shaped bay with depths of 3¼ to 8 fathoms, mud bottom, which is 12.7 miles N of Cape Ommaney and about 0.9 mile S of Port Walter Light 5.

(123) **Patterson Point** (56°32.4'N., 134°38.2'W), on the W side of Chatham Strait 23 miles N of Cape Ommaney, is the E point at the entrance to Patterson Bay.

(127) **Patterson Bay** has its entrance W of Patterson Point. Constricted anchorage in 22 to 24 fathoms can be made below the wooded islet at the head and abreast a small green point on the W side formed by the debris from the cliffs above it. Several streams enter the bay. No directions are necessary, and there are apparently no dangers in the bay.

(128) **Gut Bay** is on the W side of Chatham Strait, about 34.5 miles N of Cape Ommaney.

Table of Selected Chart Notes

Corrected through NM Mar. 01/08
Corrected through LNM Mar. 04/08

HEIGHTS

Heights in feet above Mean High Water.

NOTE C

NEVA STRAIT

All aids, channels, and dangers are not shown on this chart. Use chart 17324.

NOTE F

PERIL STRAIT AND SERGIUS NARROWS

Some aids, channels, and dangers are not shown on this chart. Use chart 17323.

RADAR REFLECTORS

Radar reflectors have been placed on many floating aids to navigation. Individual radar reflector identification on these aids has been omitted from this chart.

NOTE B

SITKA AREA

Some aids, dangers, and cable areas are not shown on this chart. Use chart 17327.

CAUTION

Temporary changes or defects in aids to navigation are not indicated on this chart. See Local Notice to Mariners.

POLLUTION REPORTS

Report all spills of oil and hazardous substances to the National Response Center via 1-800-424-8802 (toll free), or to the nearest U.S. Coast Guard facility if telephone communication is impossible (33 CFR 153).

LOCAL MAGNETIC DISTURBANCE

Differences of as much as 3° from the normal variation have been reported near Cannery Point on course to Tenakee Springs.

WARNING

The prudent mariner will not rely solely on any single aid to navigation, particularly on floating aids. See U.S. Coast Guard Light List and U.S. Coast Pilot for details.

NOTE E

CAUTION

Numerous shoals in Rowan Bay and along the edges of the entrance channel cannot be adequately shown on this chart. Mariners should exercise caution and it is recommended that chart 17370 be used when navigating within the area.

LOCAL MAGNETIC DISTURBANCE

Differences of as much as 6° from the normal variation have been reported in Chatham Strait in mid-channel between latitudes 57°15'N and 57°23'N.

HORIZONTAL DATUM

The horizontal reference datum of this chart is North American Datum of 1983 (NAD 83), which for charting purposes is considered equivalent to the World Geodetic System 1984 (WGS 84). Geographic positions referred to the North American Datum of 1927 must be corrected an average of 1.281" southward and 6.375" westward to agree with this chart.

AIDS TO NAVIGATION

Consult U.S. Coast Guard Light List for supplemental information concerning aids to navigation.

NOTE A

Navigation regulations are published in Chapter 2, U.S. Coast Pilot 8. Additions or revisions to Chapter 2 are published in the Notice to Mariners. Information concerning the regulations may be obtained at the Office of the Commander, 17th Coast Guard District in Juneau, Alaska, or at the Office of the District Engineer, Corps of Engineers in Anchorage, Alaska.

Refer to charted regulation section numbers.

NOAA WEATHER RADIO BROADCASTS

The NOAA Weather Radio stations listed below provide continuous weather broadcasts. The reception range is typically 20 to 40 nautical miles from the antenna site, but can be as much as 100 nautical miles for stations at high elevations.

| | | |
|-----------------------|--------|-------------|
| Althorp Peak, AK | KZZ-86 | 162.425 MHz |
| Mt. Robert Barron, AK | KZZ-87 | 162.450 MHz |
| Mt. McArthur, AK | KZZ-95 | 162.525 MHz |
| Sukkwan I, AK | KZZ-89 | 162.425 MHz |
| Cape Fanshaw, AK | KZZ-88 | 162.425 MHz |
| Zarembo I, AK | KZZ-91 | 162.450 MHz |
| Craig, AK | KXI-80 | 162.475 MHz |
| Juneau, AK | WXJ-25 | 162.550 MHz |
| Sitka, AK | WXJ-80 | 162.550 MHz |

Mercator Projection

Scale 1:217,828 at Lat 57°00'N
North American Datum of 1983
(World Geodetic System 1984)

SOUNDINGS IN FATHOMS AT MEAN LOWER LOW WATER

FISHERY LIMIT

Fishery limit is the limit of the State of Alaska's fishery management authority (except for crabs) in accordance with Section 306(a) of the Fishery Conservation and Management Act, where that limit is seaward of the territorial sea.

CAUTION

Limitations on the use of radio signals as aids to marine navigation can be found in the U.S. Coast Guard Light Lists and National Geospatial-Intelligence Agency Publication 117.

Radio direction-finder bearings to commercial broadcasting stations are subject to error and should be used with caution.

Station positions are shown thus:

○ (Accurate location) ◦ (Approximate location)

NOTE G

CAUTION

Previously uncharted shoals and dangerous rocks have been located and others may exist in Kelp Bay. Extreme caution should be exercised when transiting this area.

LORAN-C

GENERAL EXPLANATION

LORAN-C FREQUENCY 100kHz
PULSE REPETITION INTERVAL
5990 59,900 Microseconds
7960 79,600 Microseconds
STATION TYPE DESIGNATORS: (Not individual station letter designators).
M Master
W Secondary
X Secondary
Y Secondary
Z Secondary

EXAMPLE: 7960-X

RATES ON THIS CHART

Loran-C correction tables published by the National Geospatial-Intelligence Agency or others should not be used with this chart. The lines of position shown have been adjusted based on theoretically determined overland signal propagation delays. They have not been verified by comparison with survey data. Every effort has been made to meet the ¼ nautical mile accuracy criteria established by the U.S. Coast Guard. Mariners are cautioned not to rely solely on the lattices in inshore waters.

Additional information can be obtained at nauticalcharts.noaa.gov.

SOURCE DIAGRAM

The outlined areas represent the limits of the most recent hydrographic survey information that has been evaluated for charting. Surveys have been banded in this diagram by date and type of survey. Channels maintained by the U.S. Army Corps of Engineers are periodically resurveyed and are not shown on this diagram. Refer to Chapter 1, United States Coast Pilot.

VESSEL TRANSITING

The U.S. Coast Guard and the Pacific States/British Columbia Oil Spill Task Force endorse a system of voluntary measures and minimum distances from shore for certain commercial vessels transiting along the coast anywhere between Cook Inlet, Alaska and San Diego, California. See U.S. Coast Pilot 8, Chapter 3 for details.

AUTHORITIES

Hydrography and topography by the National Ocean Service, Coast Survey, with additional data from the U.S. Coast Guard and Geological Survey.

CAUTION

This chart has been corrected from the Notice to Mariners (NM) published weekly by the National Geospatial-Intelligence Agency and the Local Notice to Mariners (LNM) issued periodically by each U.S. Coast Guard district to the dates shown in the lower left hand corner. Chart updates corrected from Notice to Mariners published after the dates shown in the lower left hand corner are available at nauticalcharts.noaa.gov.

NOTE X

Within the 12-nautical mile Territorial Sea, established by Presidential Proclamation, some Federal laws apply. The Three Nautical Mile Line, previously identified as the outer limit of the territorial sea, is retained as it continues to depict the jurisdictional limit of the other laws. The 9-nautical mile Natural Resource Boundary off the Gulf coast of Florida, Texas, and Puerto Rico, and the Three Nautical Mile Line elsewhere remain in most cases the inner limit of Federal fisheries jurisdiction and the outer limit of the jurisdiction of the states. The 24-nautical mile Contiguous Zone and the 200-nautical mile Exclusive Economic Zone were established by Presidential Proclamation. Unless fixed by treaty or the U.S. Supreme Court, these maritime limits are subject to modification.

COLREGS, 80.1705 (see note A)

International Regulations for Preventing Collisions at Sea, 1972.
The entire area of this chart falls seaward of the COLREGS Demarcation Line.

ABBREVIATIONS (For complete list of Symbols and Abbreviations, see Chart No. 1.)

Aids to Navigation (lights are white unless otherwise indicated):

AERO aeronautical
Al alternating
B black
Bn beacon
C can
DIA diaphone
F fixed
Fl flashing

G green
IQ interrupted quick
Iso isophase
LT HO lighthouse
M nautical mile
m minutes
MICRO TR microwave tower
Mkr marker

Mo morse code
N nun
OBSC obscured
Oc occulting
Or orange
Q quick
R red
Ra Raif radar reflector
R Bn radiobeacon

R TR radio tower
Rot rotating
s seconds
SEC sector
St M statute miles
VQ very quick
W white
WHIS whistle
Y yellow

Bottom characteristics:

Blds boulders
bk broken
Cy clay

Co coral
G gravel
Grs grass
gy gray
h hard
M mud

Oys oysters
Rk rock
S sand

so soft
Sh shells
sy sticky

Miscellaneous:

AUTH authorized
ED existence doubtful

Obstrn obstruction
PA position approximate
Rep reported

PD position doubtful
Rep reported

Subm submerged

(1) Wreck, rock, obstruction, or shoal swept clear to the depth indicated.
(2) Rocks that cover and uncover, with heights in feet above datum of soundings.

NOTE X

Within the 12-nautical mile Territorial Sea, established by Presidential Proclamation, some Federal laws apply. The Three Nautical Mile Line, previously identified as the outer limit of the territorial sea, is retained as it continues to depict the jurisdictional limit of the other laws. The 9-nautical mile Natural Resource Boundary off the Gulf coast of Florida, Texas, and Puerto Rico, and the Three Nautical Mile Line elsewhere remain in most cases the inner limit of Federal fisheries jurisdiction and the outer limit of the jurisdiction of the states. The 24-nautical mile Contiguous Zone and the 200-nautical mile Exclusive Economic Zone were established by Presidential Proclamation. Unless fixed by treaty or the U.S. Supreme Court, these maritime limits are subject to modification.

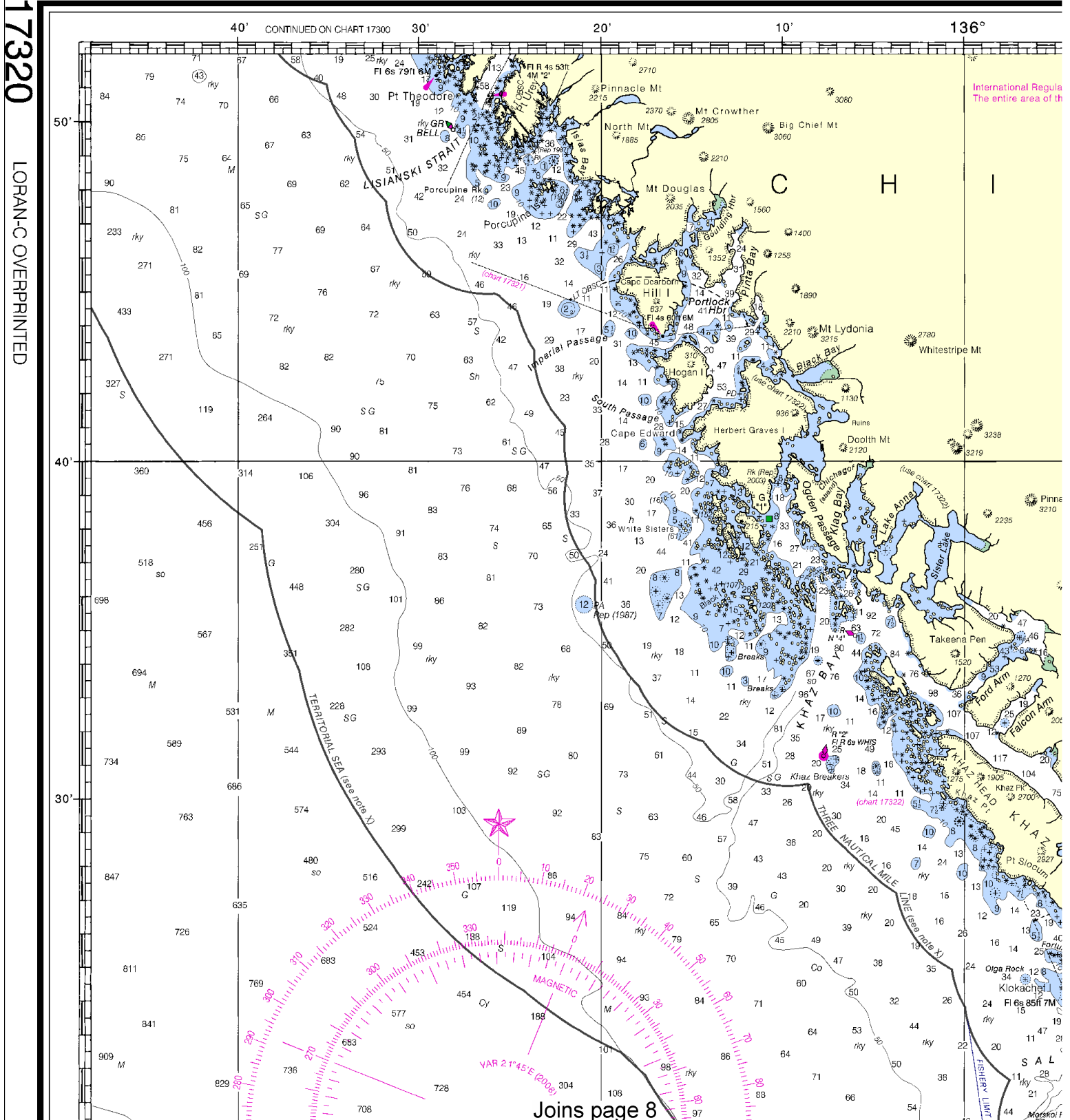
This nautical chart has been designed to promote safe navigation. The National Ocean Service encourages users to submit corrections, additions, or comments for improving this chart to the Chief, Marine Chart Division (N/CS2), National Ocean Service, NOAA, Silver Spring, Maryland 20910-3282.

PRINT-
NOAA and its partner, OceanGrafix, offer and critical corrections. Charts are printed Editions are available 5-8 weeks before the about Print-on-Demand charts or contact help@NauticalCharts.gov, or OceanG help@OceanGrafix.com.

17320

LO-RAN-C OVERPRINTED

4

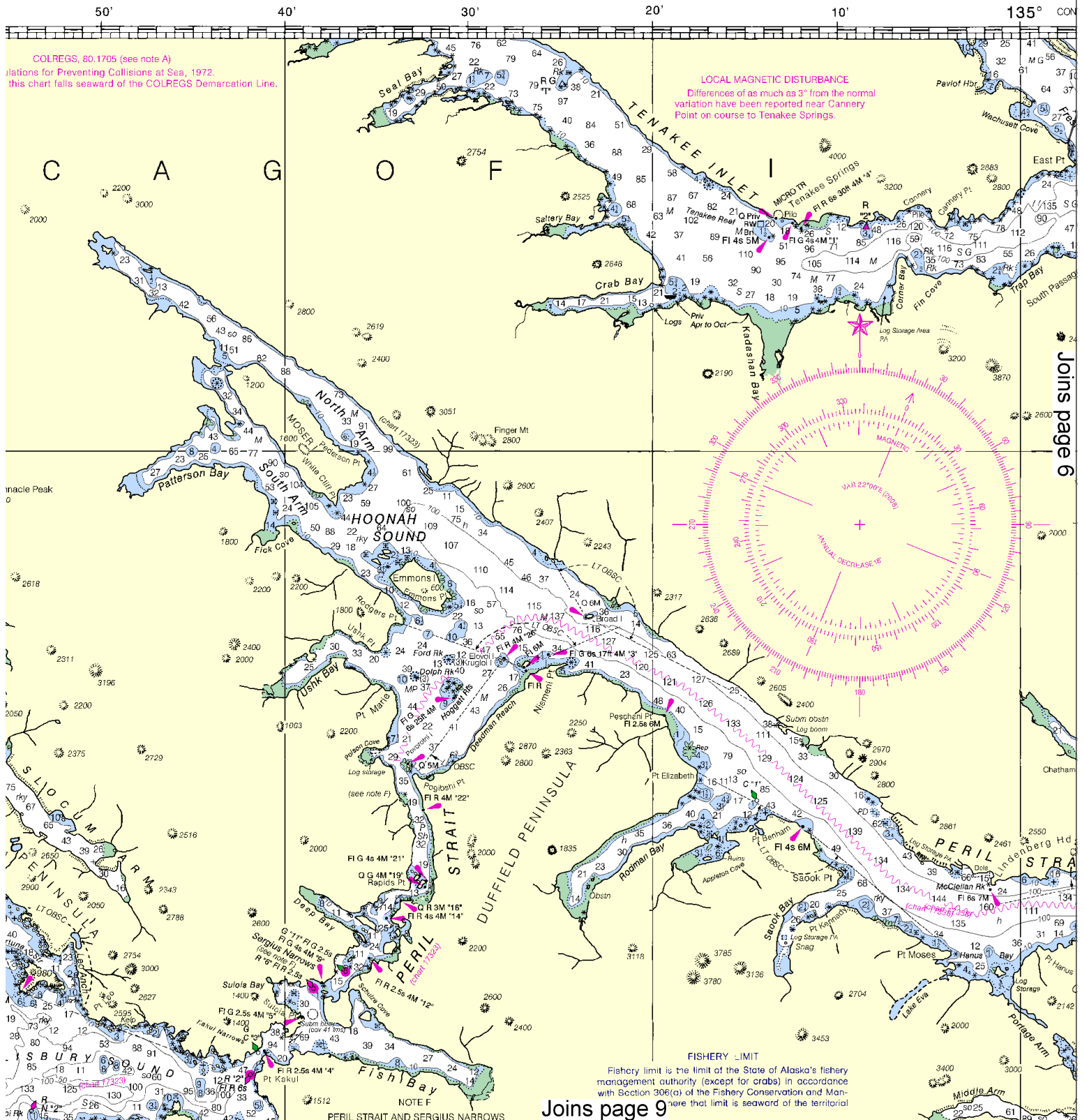


Joins page 8

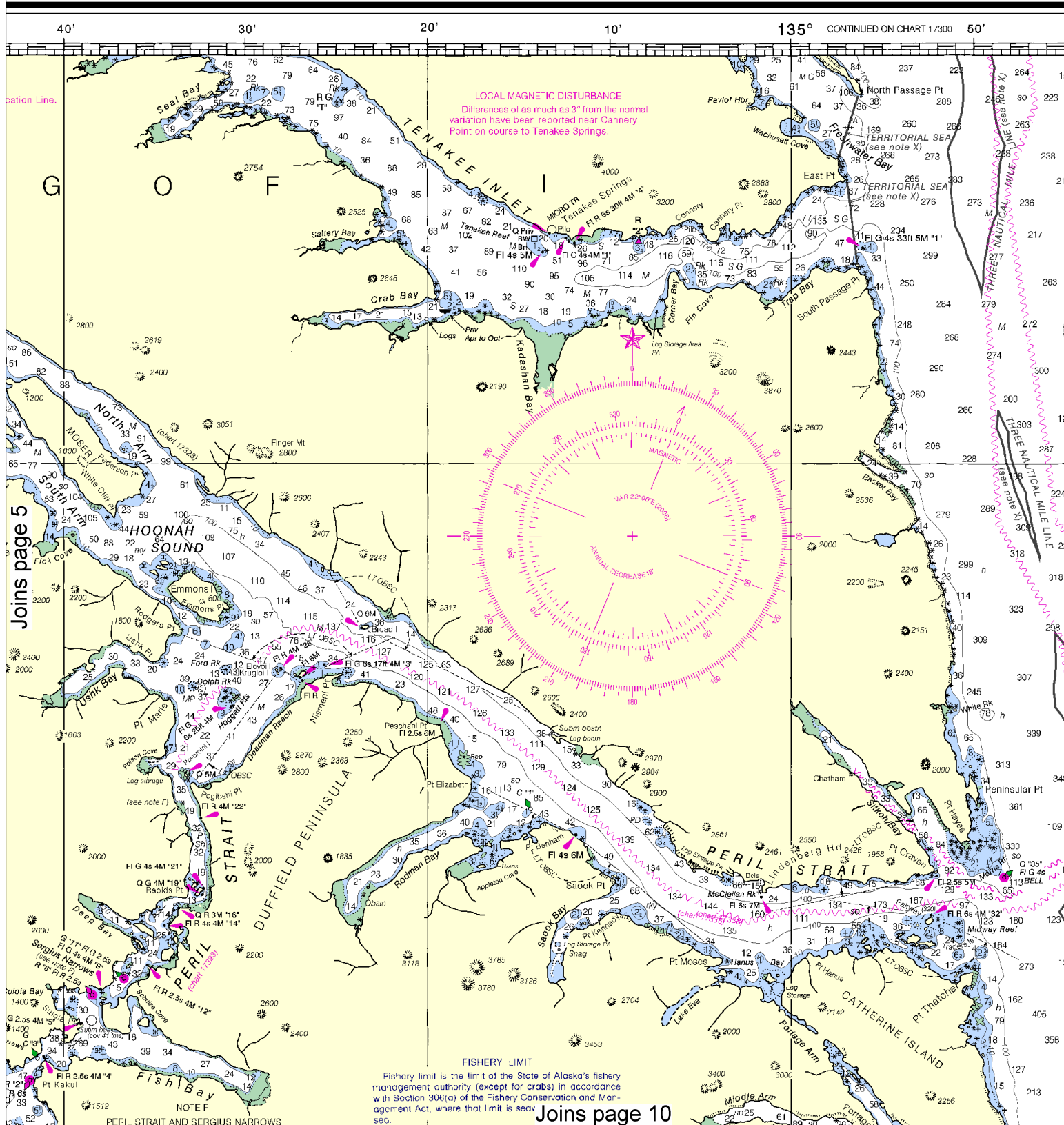
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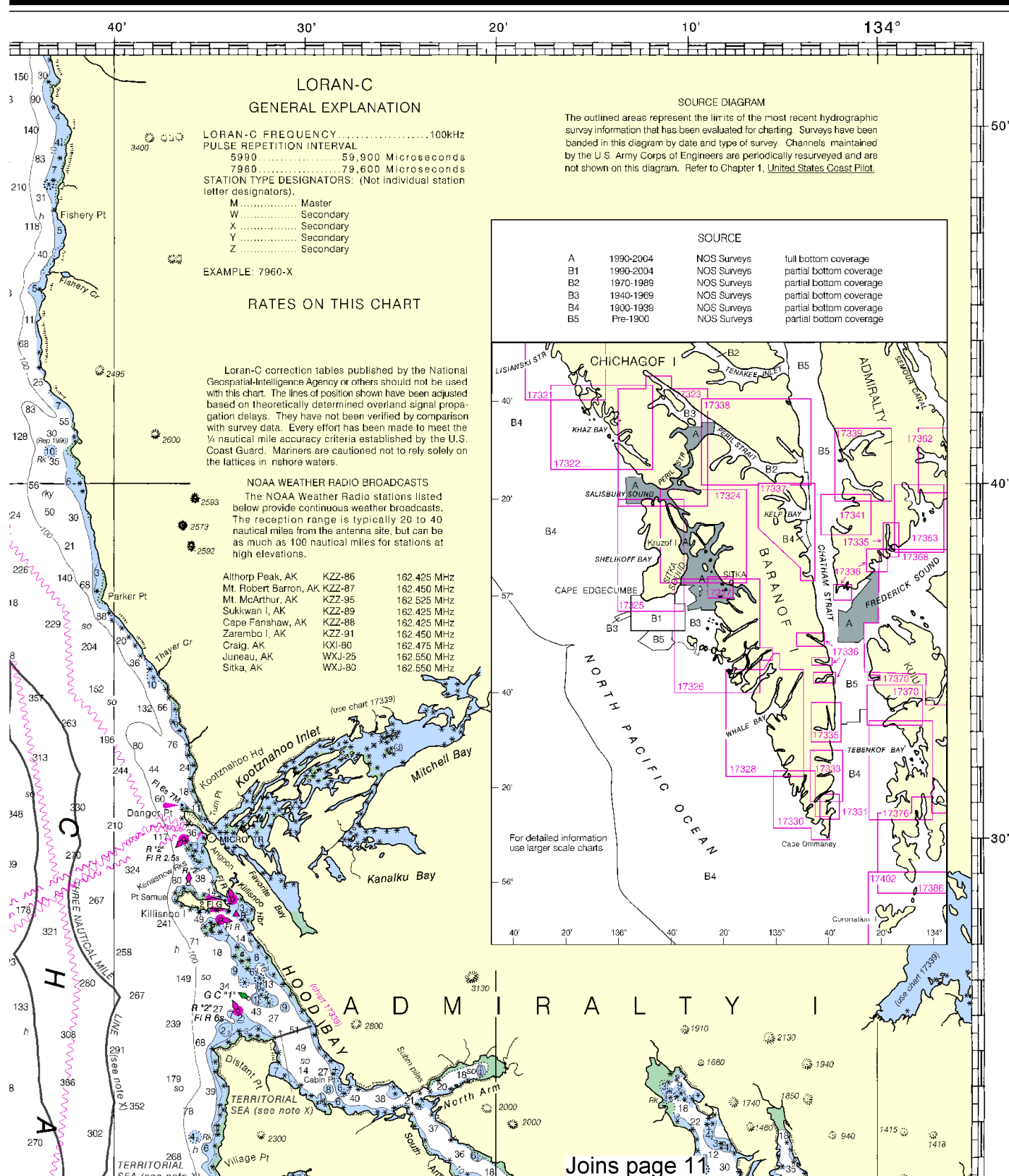
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 The new scale is 1:290437. Barscales have also been reduced and
 are accurate when used to measure distances in this BookletChart.



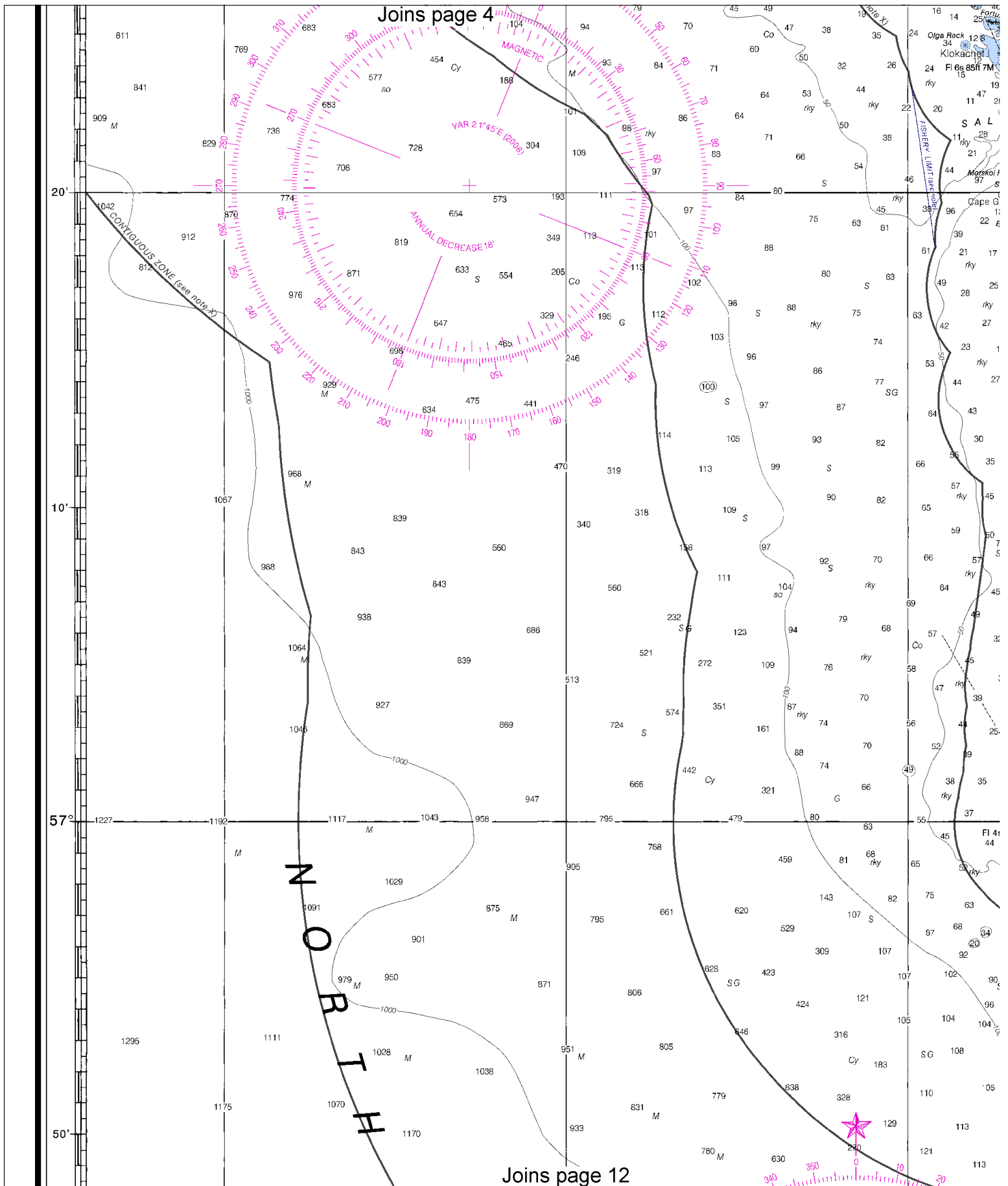
SOUNDINGS IN FATHOMS

17320

LORAN-C OVERPRINTED



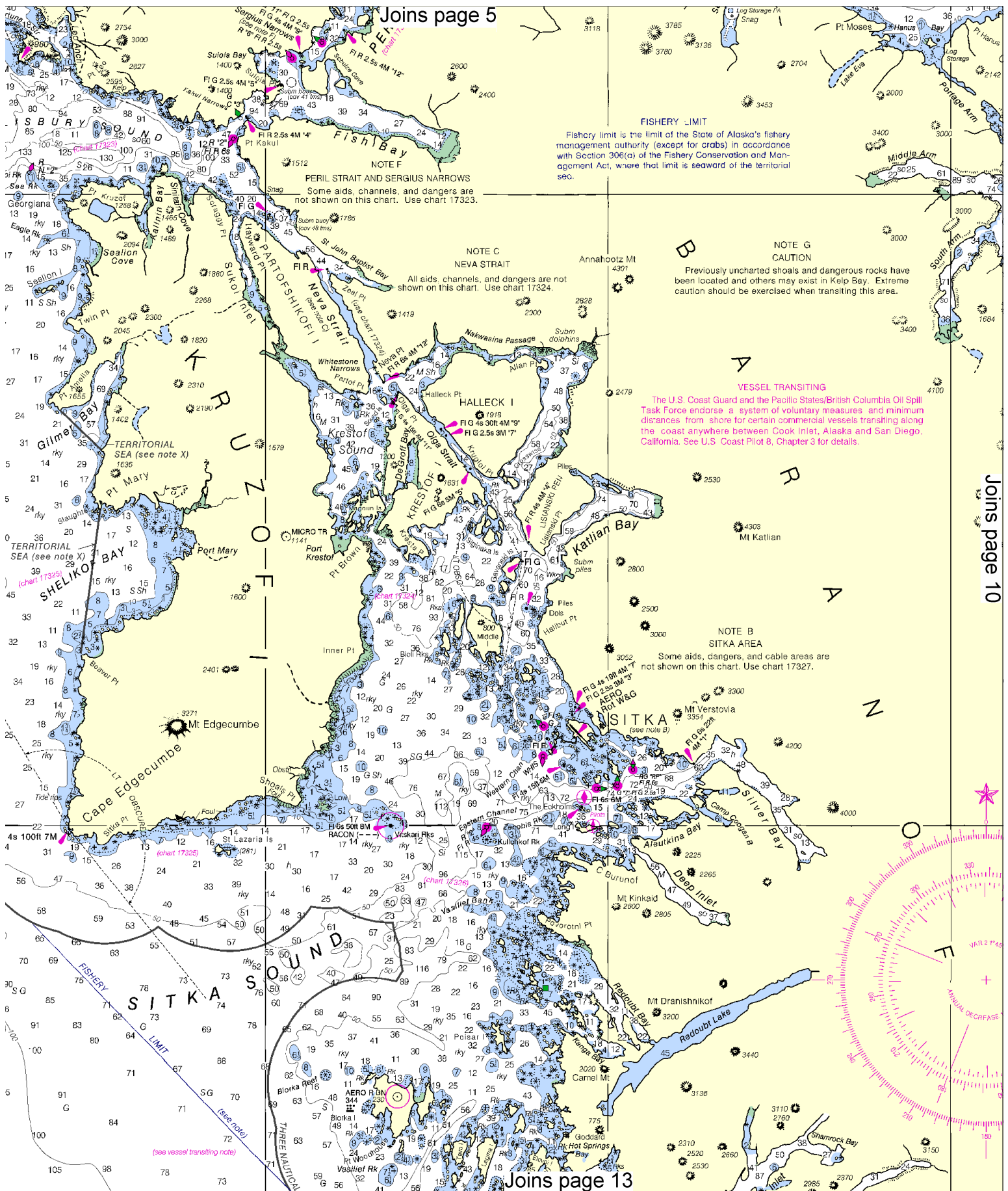
Joins page 4



Joins page 12

8

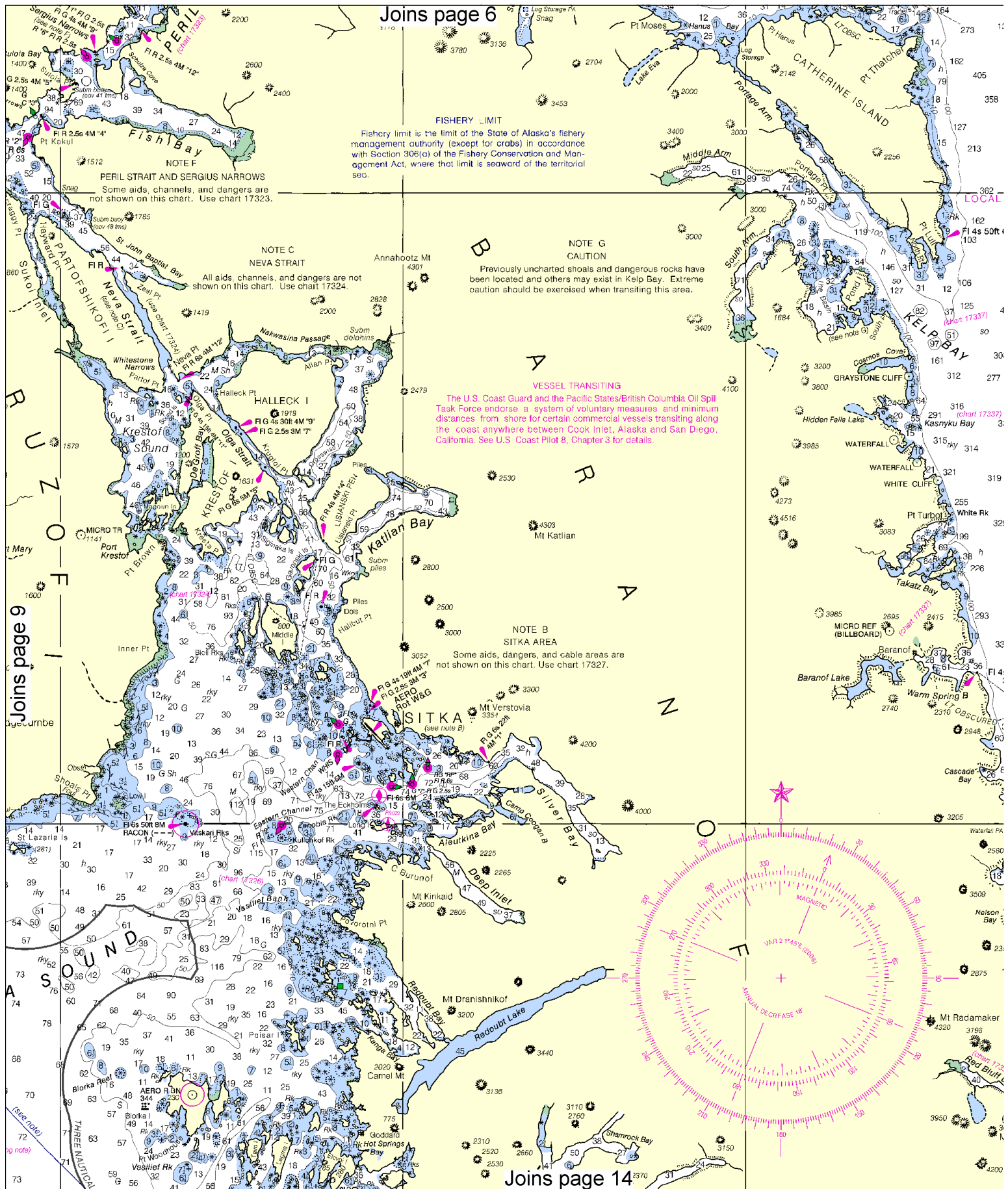




Joins page 5

Joins page 10

Joins page 13



Joins page 6

FISHERY LIMIT

Fishery limit is the limit of the State of Alaska's fishery management authority (except for crabs) in accordance with Section 306(a) of the Fishery Conservation and Management Act, where that limit is seaward of the territorial sec.

NOTE G CAUTION

Previously uncharted shoals and dangerous rocks have been located and others may exist in Kelp Bay. Extreme caution should be exercised when transiting this area.

VESSEL TRANSITING

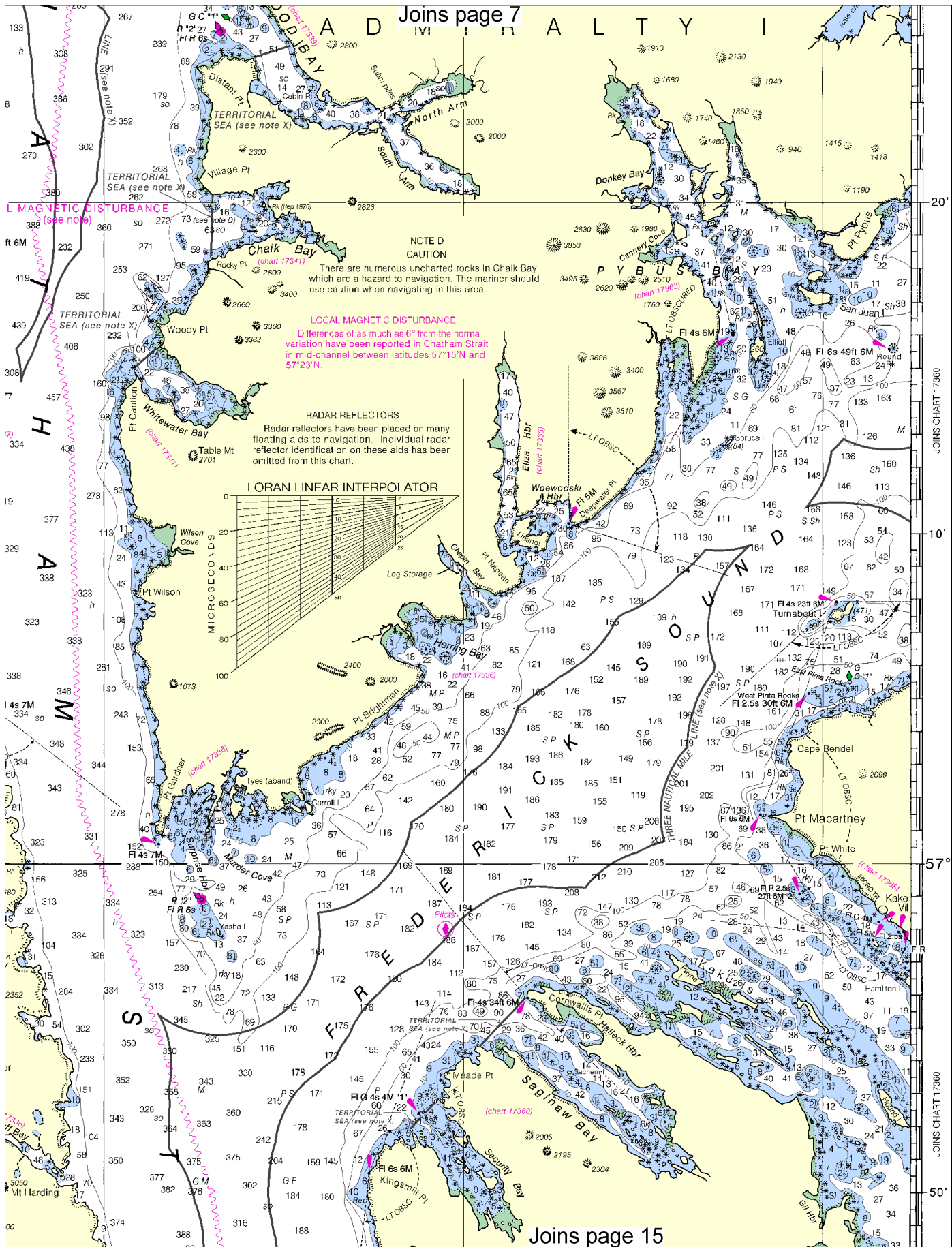
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NOTE B SITKA AREA

Some aids, dangers, and cable areas are not shown on this chart. Use chart 17327.

Joins page 14

Joins page 9



JOINS page 8

PACIFIC OCEAN

JOINS page 16

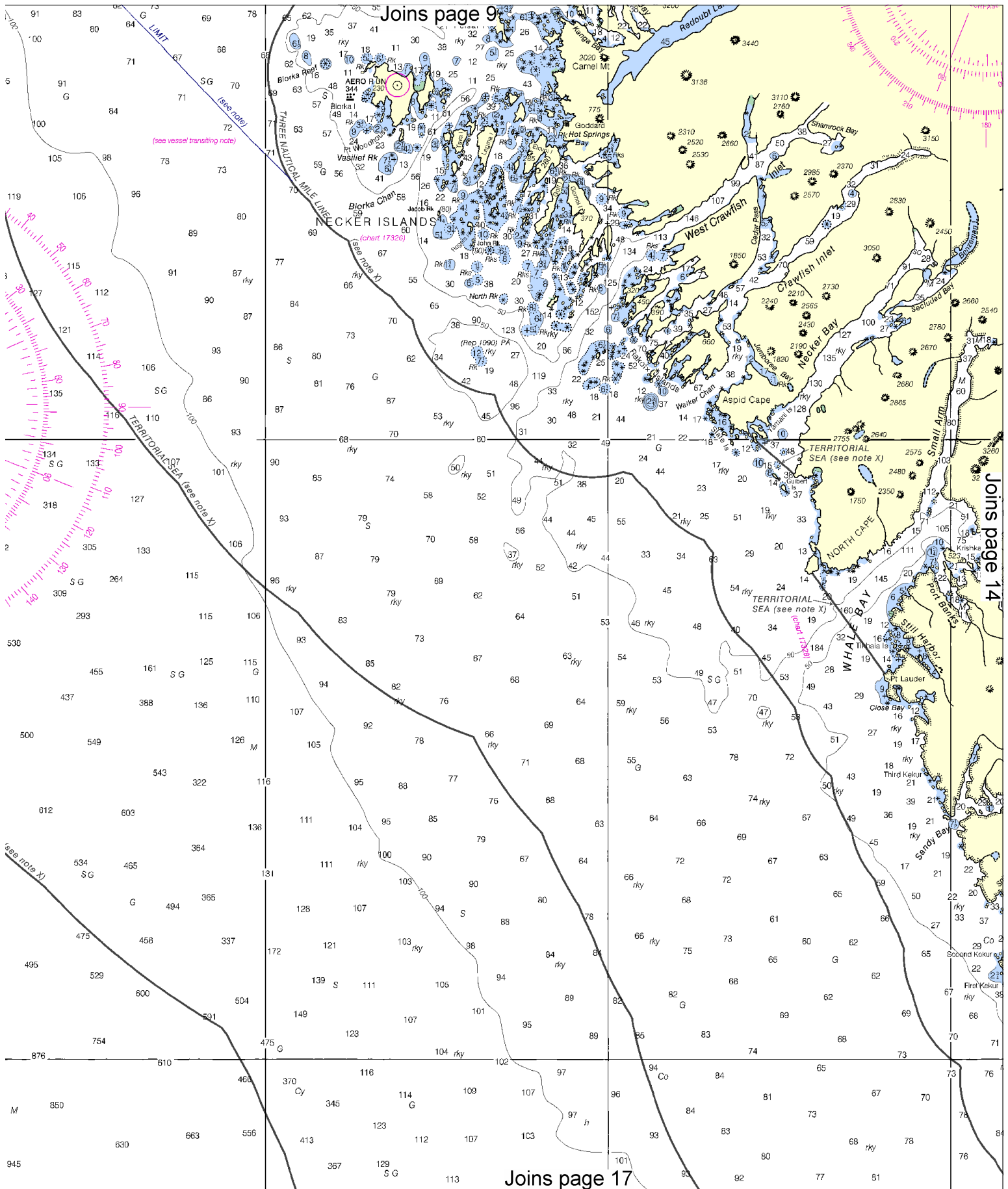
LORAN LINEAR INTERPOLATOR

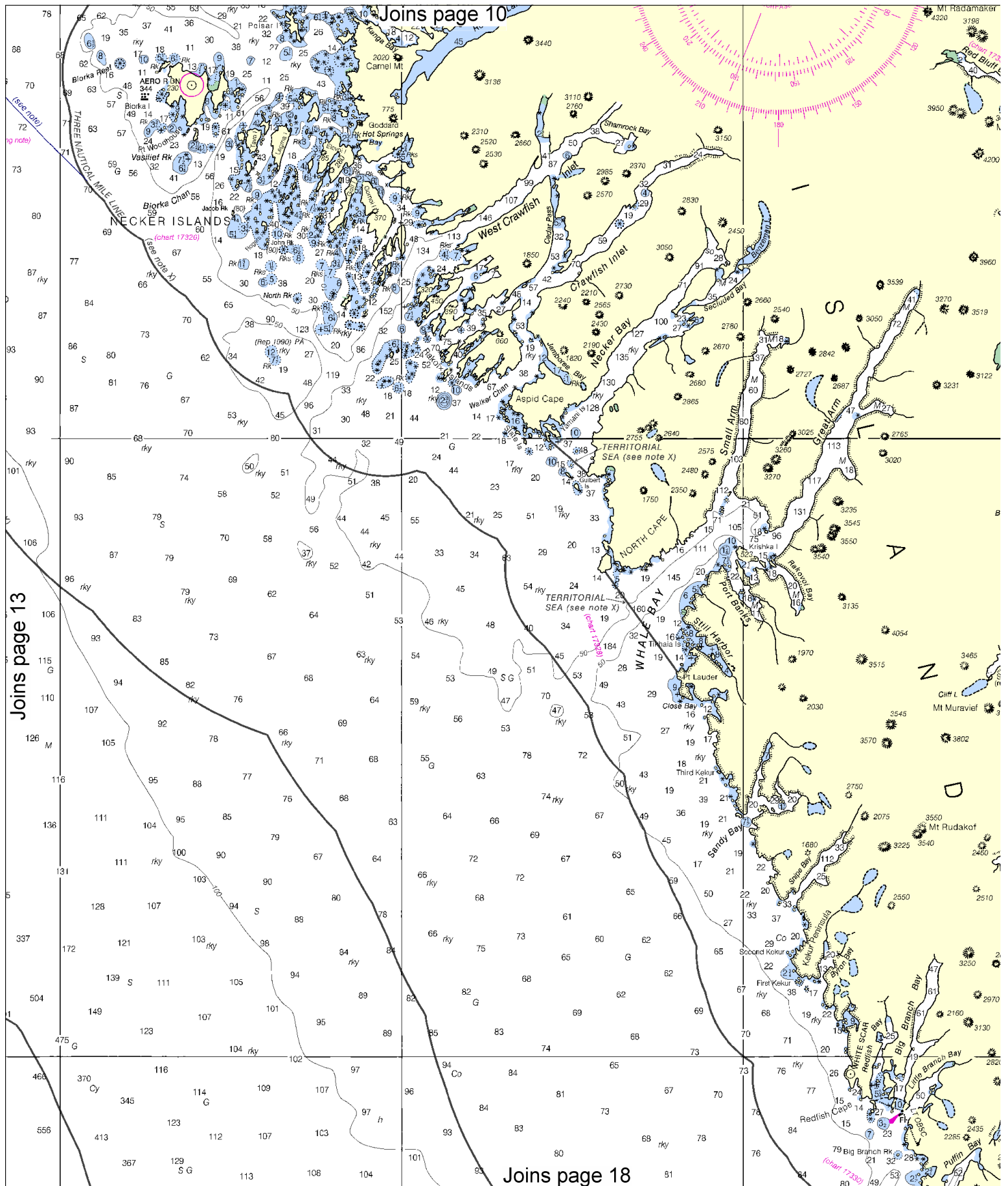
ABBREVIATIONS (For complete list of Symbols and Abbreviations, see Chart No. 1.)
Aids to Navigation (lights are white unless otherwise indicated):

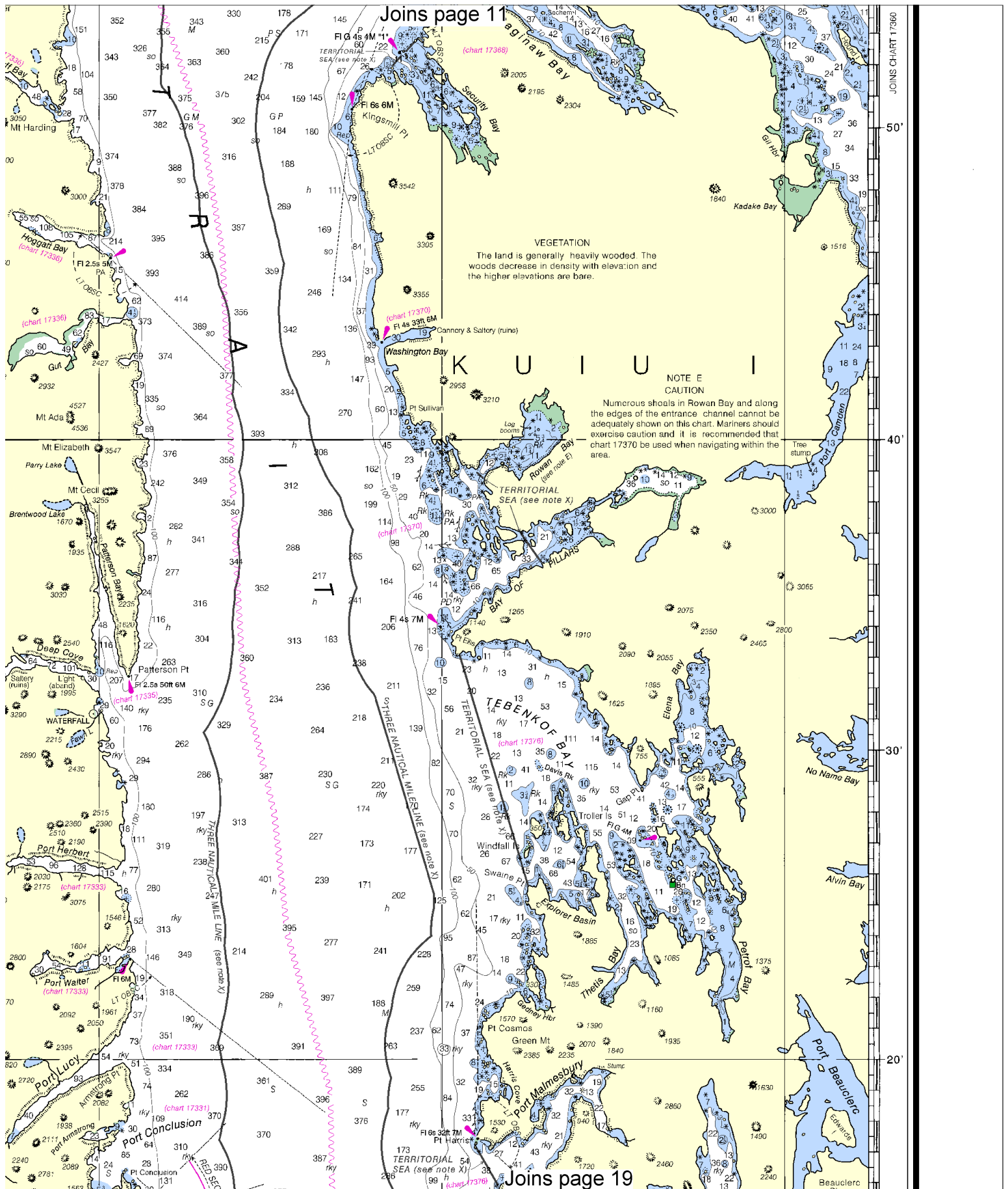
| | | | |
|-------------------|--------------------------|---------------|-----------------------|
| AERO aeronautical | G green | Mo morse code | R TR radio tower |
| Al alternating | IC interrupted quick | N run | Rot rotating |
| B black | Is isophase | OBSC obscured | s seconds |
| Bn beacon | LT LHO lighthouse | Oc occulting | SEC sector |
| C can | M nautical mile | Or orange | St M statute miles |
| DIA diaphone | m minutes | Q quick | VC very quick |
| F fixed | MICRO TR microwave tower | R red | W wave |
| Fl flash | Mo microwave | Rd red | Re-Rd radar collector |
| | | | |

12









Joins page 11

JOINS CHART 17360

50'

40'

30'

20'

VEGETATION
The land is generally heavily wooded. The woods decrease in density with elevation and the higher elevations are bare.

NOTE E
CAUTION
Numerous shoals in Rowan Bay and along the edges of the entrance channel cannot be adequately shown on this chart. Mariners should exercise caution and it is recommended that chart 17370 be used when navigating within the area.

Joins page 19

LORAN LINEAR INTERPOLATOR

ABBREVIATIONS (For complete list of Symbols and Abbreviations, see Chart No. 1.)

Aids to Navigation (lights are white unless otherwise indicated):

| | | | |
|-------------------|--------------------------|------------------------|--------------------|
| AERO aeronautical | G green | Mo mouse code | R TR radio tower |
| Al alternating | IQ interrupted quick | N nun | Rot rotating |
| B black | Is isophase | OBSC obscured | s seconds |
| Bn beacon | LT LHO lighthouse | Oc occulting | SEC sector |
| C can | M nautical mile | Or orange | St M statute miles |
| DIA diaphone | m minutes | Q quick | VQ very quick |
| F fixed | MICRO TR microwave tower | R red | W white |
| Fl flashing | Mkr marker | Re Ref radar reflector | WHIS whistle |
| | | R Bn radiobeacon | Y yellow |

Bottom characteristics:

| | | | | |
|--------------|-----------|---------|-------------|-----------|
| Bls boulders | Co coral | gy gray | Oys oysters | so soft |
| bk broken | G gravel | h hard | Rk rock | Sh shells |
| Cy clay | Grs grass | M mud | S sand | sy sticky |

Miscellaneous:

| | | | |
|-----------------------|-------------------------|----------------------|----------------|
| AUTH authorized | Obstr obstruction | PD position doubtful | Subm submerged |
| ED existence doubtful | PA position approximate | Rap reported | |

(2) Wreck, rock, obstruction, or shoal swept clear to the depth indicated.

(2) Rocks that cover and uncover, with heights in feet above datum of soundings.

HEIGHTS

Heights in feet above Mean High Water.

AUTHORITIES

Hydrography and topography by the National Ocean Service, Coast Survey, with additional data from the U.S. Coast Guard and Geological Survey.

HORIZONTAL DATUM

The horizontal reference datum of this chart is North American Datum of 1983 (NAD 83), which for charting purposes is considered equivalent to the World Geodetic System 1984 (WGS 84). Geographic positions referred to the North American Datum of 1927 must be corrected an average of 1.261" southward and 6.375" westward to agree with this chart.

WARNING

The prudent mariner will not rely solely on any single aid to navigation, particularly on floating aids. See U.S. Coast Guard Light List and U.S. Coast Pilot for details.

AIDS TO NAVIGATION

Consult U.S. Coast Guard Light List for supplemental information concerning aids to navigation.

CAUTION

Limitations on the use of radio signals as aids to marine navigation can be found in the U.S. Coast Guard Light Lists and National Geospatial-Intelligence Agency Publication 117.

Radio direction-finder bearings to commercial broadcasting stations are subject to error and should be used with caution.

Station positions are shown thus:

⊙ (Accurate location) ⊙ (Approximate location)

NOTE A

Navigation regulations are published in Chapter 2, U.S. Coast Pilot 8. Additions or revisions to Chapter 2 are published in the Notice to Mariners. Information concerning the regulations may be obtained at the Office of the Commander, 17th Coast Guard District in Juneau, Alaska, or at the Office of the District Engineer, Corps of Engineers in Anchorage, Alaska.

Refer to charted regulation section numbers.

CAUTION

Temporary changes or defects in aids to navigation are not indicated on this chart. See Local Notice to Mariners.

POLLUTION REPORTS

Report all spills of oil and hazardous substances to the National Response Center via 1-800-424-8802 (toll free), or to the nearest U.S. Coast Guard facility if telephone communication is impossible (33 CFR 153).



THE NATION'S CHARTMAKER SINCE 1807

UNITED STATES

ALASKA - SOUTHEAST COAST

CORONATION ISLAND TO LISIANSKI S

Mercator Projection
Scale 1:217,828 at Lat 57°00'N
North American Datum of 1983
(World Geodetic System 1984)

SOUNDINGS IN FATHOMS
AT MEAN LOWER LOW WATER

Additional information can be obtained at nauticalcharts.noaa.gov.

18th Ed., Mar./08 ■ Corrected through NM Mar. 01/08
Corrected through LNM Mar. 04/08

17320

LORAN-C OVERPRINTED

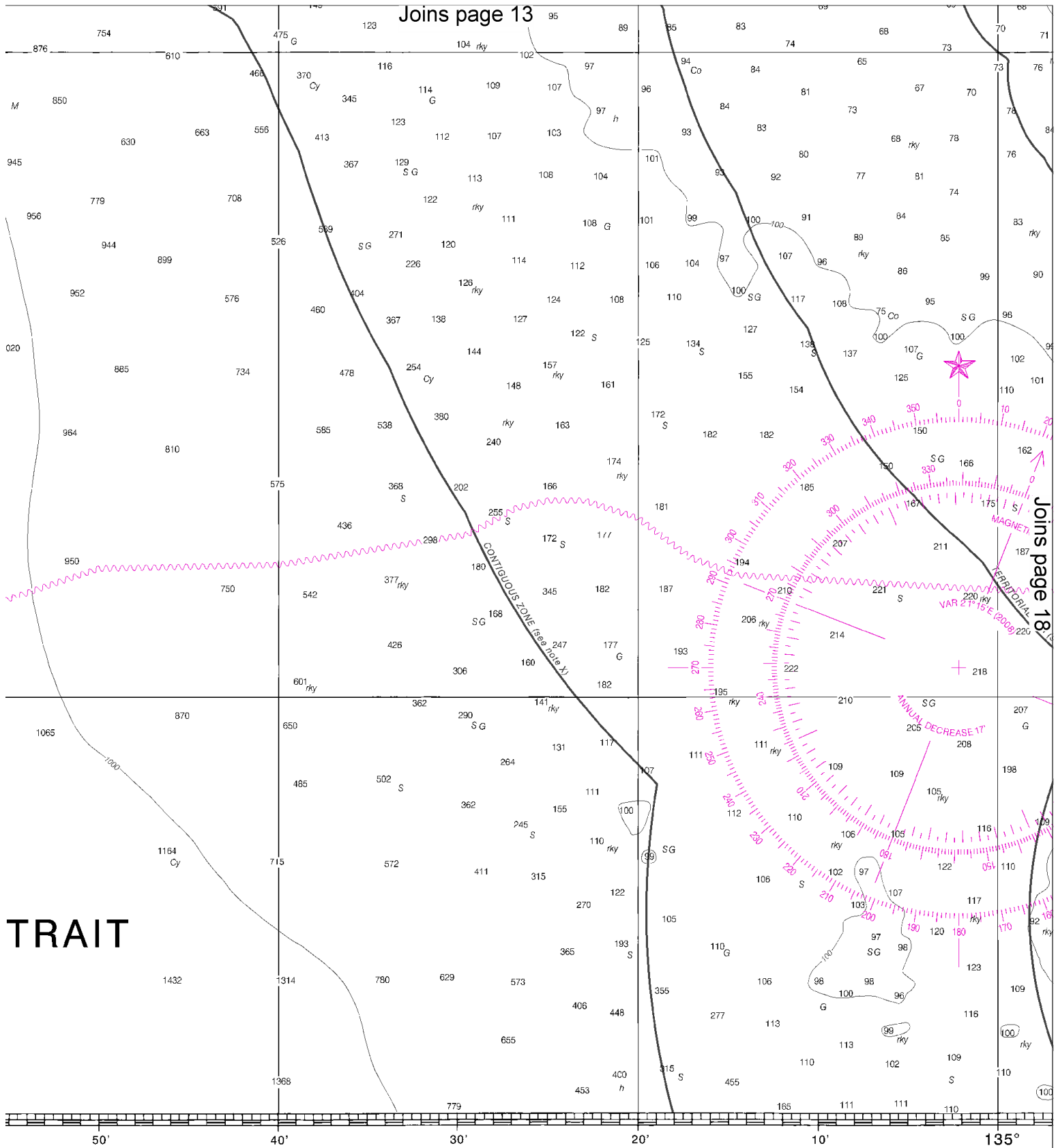
CAUTION

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SOUNDINGS IN FA

16



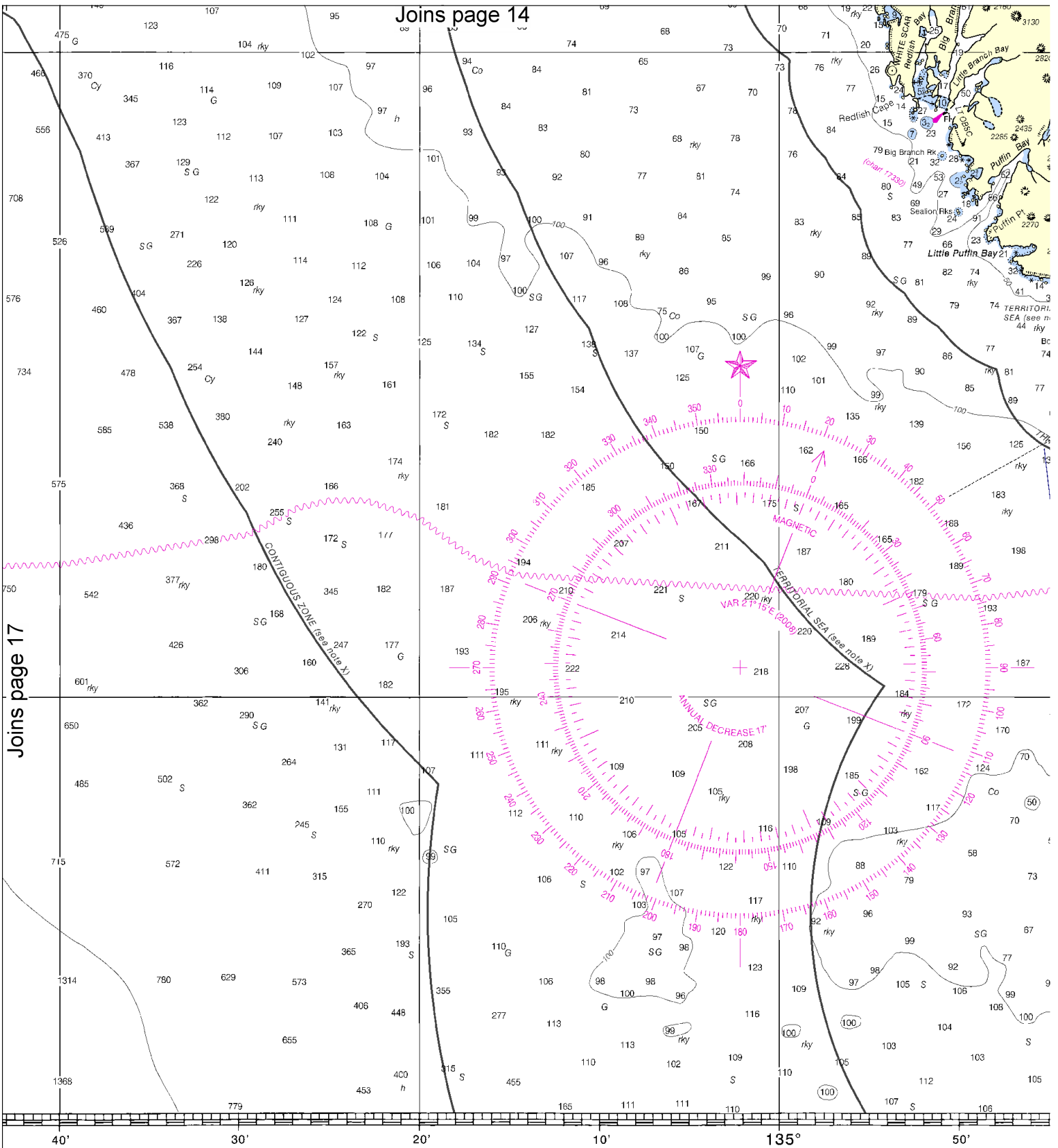


TRAIT

ATHOMS

Published at Washington, D.C.
U.S. DEPARTMENT OF COMMERCE
NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION
NATIONAL OCEAN SERVICE
COAST SURVEY

| | |
|---------|-----|
| FATHOMS | 1 |
| FEET | 6 |
| METERS | 1.2 |



Published at Washington, D.C.
U.S. DEPARTMENT OF COMMERCE
NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION
NATIONAL OCEAN SERVICE
COAST SURVEY

| FATHOMS | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | 19 | 20 |
|---------|---|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|-----|-----|-----|-----|
| FEET | 6 | 12 | 18 | 24 | 30 | 36 | 42 | 48 | 54 | 60 | 66 | 72 | 78 | 84 | 90 | 96 | 102 | 108 | 114 | 120 |
| METERS | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | 19 | 20 |



EMERGENCY INFORMATION

VHF Marine Radio channels for use on the waterways:

Channel 6 – Inter-ship safety communications.

Channel 9 – Communications between boats and ship-to-coast.

Channel 13 – Navigation purposes at bridges, locks, and harbors.

Channel 16 – Emergency, distress and safety calls to Coast Guard and others, and to initiate calls to other vessels. Contact the other vessel, agree to another channel, and then switch.

Channel 22A – Calls between the Coast Guard and the public. Severe weather warnings, hazards to navigation and safety warnings are broadcast here.

Channels 68, 69, 71, 72 & 78A – Recreational boat channels.

Distress Call Procedures

1. Make sure radio is on.
2. Select Channel 16.
3. Press/Hold the transmit button.
4. Clearly say: "MAYDAY, MAYDAY, MAYDAY."
5. Also give: Vessel Name and/or Description; Position and/or Location; Nature of Emergency; Number of People on Board.
6. Release transmit button.
7. Wait for 10 seconds – If no response Repeat MAYDAY Call.

HAVE ALL PERSONS PUT ON LIFE JACKETS !!

Mobile Phones – Call 911 for water rescue.

Coast Guard Search & Rescue (Pacific Coord) – 510-437-3700

Coast Guard Search & Rescue (RCC Juneau) – 907-463-2000

NOAA Weather Radio – 162.400 MHz, 162.425 MHz, 162.450 MHz, 162.475 MHz, 162.500 MHz, 162.525 MHz, 162.550 MHz.

Getting and Giving Help – Signal other boaters using visual distress signals (flares, orange flag, lights, arm signals); whistles; horns; and on your VHF radio. You are required by law to help boaters in trouble. Respond to distress signals, but do not endanger yourself.



NOAA CHARTING PUBLICATIONS

Official NOAA Nautical Charts – NOAA surveys and charts the national and territorial waters of the U.S, including the Great Lakes. We produce over 1,000 traditional nautical charts covering 3.4 million square nautical miles. Carriage of official NOAA charts is mandatory on the commercial ships that carry our commerce. They are used on every Navy and Coast Guard ship, fishing and passenger vessels, and are widely carried by recreational boaters. NOAA charts are available from official chart agents listed at: www.NauticalCharts.NOAA.gov.

Official Print-on-Demand Nautical Charts – These full-scale NOAA charts are updated weekly by NOAA for all Notice to Mariner corrections. They have additional information added in the margin to supplement the chart. Print-on-Demand charts meet all federal chart carriage regulations for charts and updating. Produced under a public/private partnership between NOAA and OceanGrafix, LLC, suppliers of these premium charts are listed at www.OceanGrafix.com.

Official Electronic Navigational Charts (NOAA ENC[®]) – ENCs are digital files of each chart's features and their attributes for use in computer-based navigation systems. ENCs comply with standards of the International Hydrographic Organization. ENCs and their updates are available for free from NOAA at www.NauticalCharts.NOAA.gov.

Official Raster Navigational Charts (NOAA RNC[™]) – RNCs are geo-referenced digital pictures of NOAA's charts that are suitable for use in computer-based navigation systems. RNCs comply with standards of the International Hydrographic Organization. RNCs and their updates are available for free from NOAA at www.NauticalCharts.NOAA.gov.

Official BookletCharts[™] – BookletCharts[™] are reduced scale NOAA charts organized in page-sized pieces. The "Home Edition" can be downloaded from NOAA for free and printed. The Internet address is www.NauticalCharts.gov/bookletcharts.

Official PocketCharts[™] – PocketCharts[™] are for beginning recreational boaters to use for planning and locating, but not for real navigation. Measuring a convenient 13" by 19", they have a 1/3 scale chart on one side, and safety, boating, and educational information on the reverse. They can be purchased at retail outlets and on the Internet.

Official U.S. Coast Pilot[®] – The Coast Pilots are 9 text volumes containing information important to navigators such as channel descriptions, port facilities, anchorages, bridge and cable clearances, currents, prominent features, weather, dangers, and Federal Regulations. They supplement the charts and are available from NOAA chart agents or may be downloaded for free at www.NauticalCharts.NOAA.gov.

Official On-Line Chart Viewer – All NOAA nautical charts are viewable here on-line using any Internet browser. Each chart is up-to-date with the most recent Notices to Mariners. Use these on-line charts as a ready reference or planning tool. The Internet address is www.NauticalCharts.gov/viewer.

Official Nautical Chart Catalogs – Large format, regional catalogs are available for free from official chart agents. Page size, state catalogs are posted on the Internet and can be printed at home for free. Go to <http://NauticalCharts.NOAA.gov/mcd/ccatalogs.htm>.

Internet Sites: www.NauticalCharts.NOAA.gov, www.NOAA.gov, www.TidesandCurrents.NOAA.gov, www.NOS.NOAA.gov.